



Fig. 13-D Indian - D-7 Cat parked adjacent to Indian Road.

## 24. SPORTSMAN'S LAKE ROAD

### Location

The southern terminus of this project is located at mile post 98.7 on the Kenai Highway. This point is approximately 2½ miles northwest of the intersection of the Kenai and Sterling Highways at Soldotna. The road then extends in a northerly direction for a distance of 1.2 miles and terminates at Sportsman Lake.

### Physical Data

The estimated clearing and stripping width is 90 feet and the roadbed width is 24 feet shoulder to shoulder.

The road traverses flat topography; the amount of side and cut borrow is very minor. The roadbed is essentially built with material from a borrow pit and placed in a 1½-foot lift. The borrow source is believed to be a pit located adjacent to the Kenai Highway and 1.2 miles east of the subject project.

The project was completed prior to the writer's visit (October 15) and there was no equipment located on the job site. However, the following construction equipment was parked in the Bureau of Public Roads depot at Soldotna:

- 7 - Tractors with dozers (D-7 and TD-18)
- 5 - Caterpillar bottom dumps
- 2 - LeTourneau turnpills with scrapers
- 3 - Scrapers

This project is approximately 152 miles via the Sterling Highway to Anchorage and also accessible to contractors located on the Kenai Peninsula.



Fig. 24-A Sportsman's Lake Road -  
end of road looking back.



Fig. 24-B Sportswoman's Lake Road -  
typical section.



## 25. COHOE ROAD

### Location

At approximately mile post 115 on the Sterling Highway, an existing spur extends from the highway in a northerly direction to the ocean beach. There are three roads branching off from this spur.

- 1) At a point approximately 2.4 miles from the Sterling Highway the Russell and Webb Road branches off to the Northeast (See Section 26).
- 2) At a point approximately 6.9 miles from the Sterling Highway a spur to the town of Coho branches off to the Northeast. This road appeared to have no work done on it this season, with the possible exception of light maintenance.
- 3) At a point approximately 7 miles from the Sterling Highway, the subject road branches off to the West and follows the coastline in a southwesterly direction. Apparently it is planned that this road will tie back into the Sterling Highway in the vicinity of Glam Gulch.

### Physical Data

The grade has been constructed for a distance of about 4.6 miles, and clearing and stripping have been reputedly accomplished for an additional 2½ miles. The clearing and stripping width is approximately 75 feet and the roadbed width is 26 feet shoulder to shoulder.

The road traverses flat topography on the bench above the sea cliff and the gradients are light. The earthwork is essentially a borrow pit operation. A small amount of in situ material from V ditches is utilized for subgrade and then approximately 1 foot of gravelly sand is placed as a blanket course. The only pit noted is located near the center of the project and adjacent to the right-of-way.

This project was shut down at the time of the writer's visit. Accordingly, the construction period and equipment used are unknown.

The standards to which this road is built are similar to a statewide county (farm to market) road.

The project is accessible by highway from Anchorage and there are numerous small contractors on the Kenai Peninsula.



Fig. 23-A Coboc Road - End of passable road.



Fig. 25-B

Cohse Road - Gravel pit  
located near center of  
project.

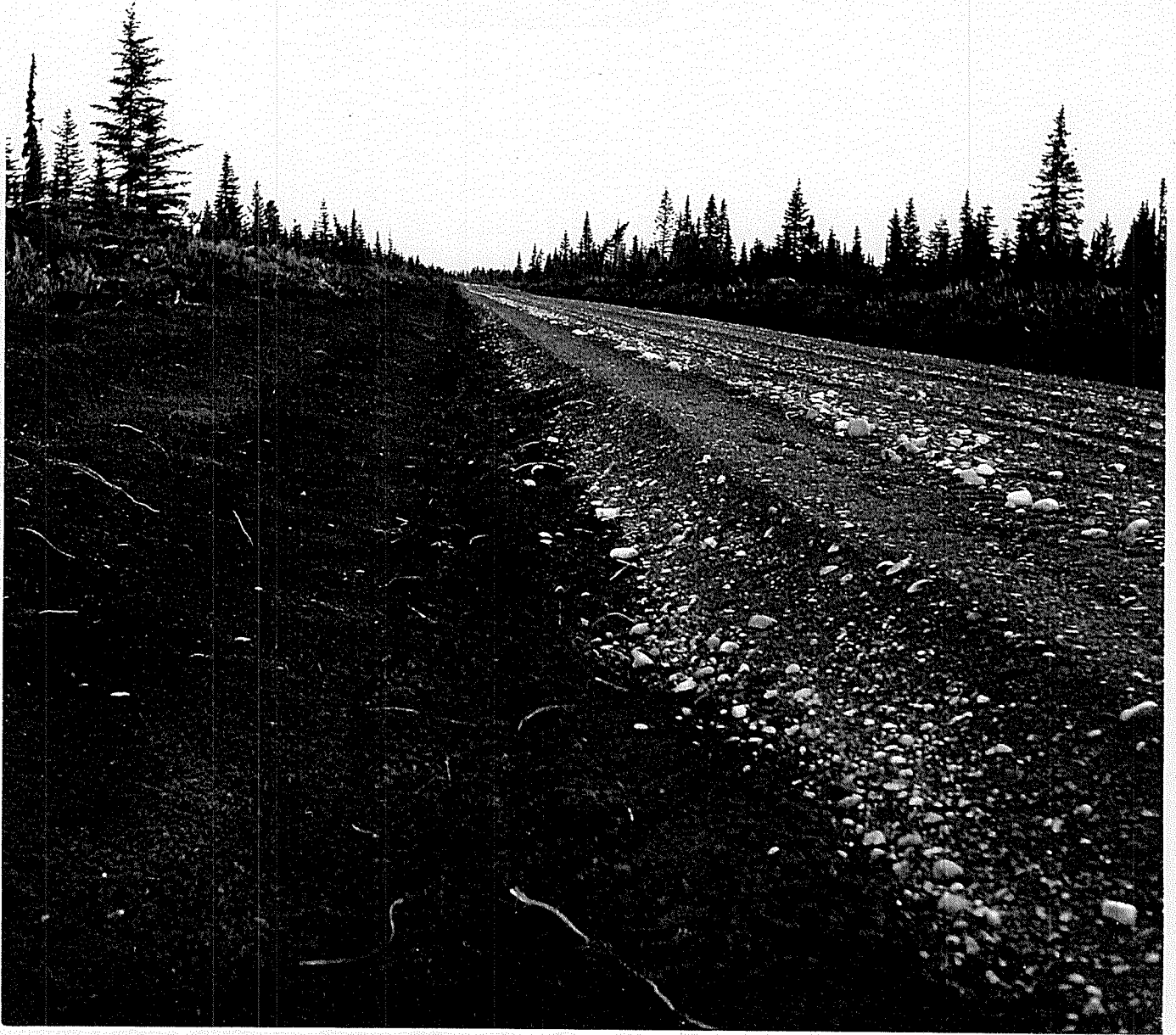


FIG. 25-G Coboc Road - Typical section.



26. RANSSELL & WHEED ROADLocation

The western terminus of this road is on the spur from the Sterling Highway and the ocean beach south of the village of Coho (see Section 35). The western terminus is approximately 2.4 miles from the Sterling Highway; thence in an easterly and northerly direction to dead end at farm houses.

According to a local farmer whose farm is serviced by this road, approximately 2 miles were resurfaced with a thin gravel surfacing approximately 20 feet in width.



Fig. 20-A Ransell & Webb Road -  
Typical section.

27. CLAM GULCH ROADLocation

The eastern terminus of this project is located near mile post 121 on the Sterling Highway; thence Northwesterly down the sea cliff to the ocean beach.

The project is 0.7 mile long and approximately 20 feet wide. It appears that this road received a new gravel surfacing which varies in thickness from an estimated 4 inches to 1 foot.

There was no activity on the project during the visit. Therefore, the time of construction and equipment used are unknown.

Design standards are low because it appears that the road is intended as an access road to the beach for fishermen.



Fig. 27-A Giam Gulch Road - typical section.



28. WHIRLEY GULCH ROADLocation

The eastern terminus of this project is located at mile post 134.8 on the Sterling Highway; thence northwesterly down the sea cliff to the ocean beach. This road is apparently an access road to the beach for fishermen and thus, the design standards are low.

Physical Data

The road is approximately 0.4 mile long and varies from 14 to 20 feet in width. As you will note in the attached photographs, the major part of the road is a side hill section coming down the sea cliff. Therefore, the gradient is steep (maximum 10-12%) and the depths of cut and fill at the shoulder line are high. The maximum estimated cut on the uphill side is approximately 20 feet and a maximum fill of 30 feet on the downhill side.

There was no construction activity during the writer's visit. However, it was reported that this project was constructed by the Bureau of Public Roads this season.

It will be noted in the pictures that portions of the road, especially the fill slopes, are already in need of maintenance.

The embankments are constructed with the in situ silty sand from cuts and surfaced with 6 inches of gravelly sand, apparently from the beach.



to see - South (left) of the road -  
road - dead end of road  
entire road in vicinity

**Fig. 20-A Whiskey Gulch Road -**  
**end of road.**



Fig. 20-8 — Whiskey Gulch Road — end of road looking back. Almost entire road is visible.





**Fig. 20-0** - Whiskey Gulch Road -  
maximum fill, no erosion.





Fig. 28-b Whiskey Gulch Road - end of road looking west.

29. NORTH FORK ROADLocation

The point of beginning of this project is at mile post 158.1 on the Sterling Highway; thence in a northeasterly direction along the general route of the North Fork of the Anchor River. The planned eastern terminus of this route is unknown.

Physical Data

The section of road worked on this season appears to be approximately 0.6 miles in length, beginning at a point approximately 0.8 mile from the intersection with the Sterling Highway. The grade is constructed for approximately 4.6 miles and clearing and stripping are reportedly completed for an additional 2 miles. The clearing width is approximately 75 feet and the roadbed width varies from 18 feet to 22 feet, shoulder to shoulder. The topography may be classified as flat, with the exception of creek crossing approaches, and the maximum estimated gradient is 6%.

The earthwork consists of a borrow pit operation with minor areas of cut to fill. The subgrade is built with an in situ sandy silt upon which a gravel overlay varying from 6 inches to 1½ feet in depth is placed. The estimated maximum cut is 10 feet at centerline and the maximum fill is approximately 15 feet.

The project was shut down at the time of the visit (October 15) but information obtained from an equipment operator who had worked on the job indicated that work was commenced in May and shut down in October. Most of the equipment used on the project is now in the Soldotna yard and consisted of 4 dozers, tournapulls and Caterpillar bottom dumps.

The design standards of this project are somewhat indefinite. The heavy fills have been narrowed down apparently to conserve on fill material.

This project is accessible by highway to Anchorage contractors and those that have equipment spreads on the Kenai Peninsula.