



Fig. 31-11 SS 07 None; end of passable road this date. Subgrade built for additional 7 miles.

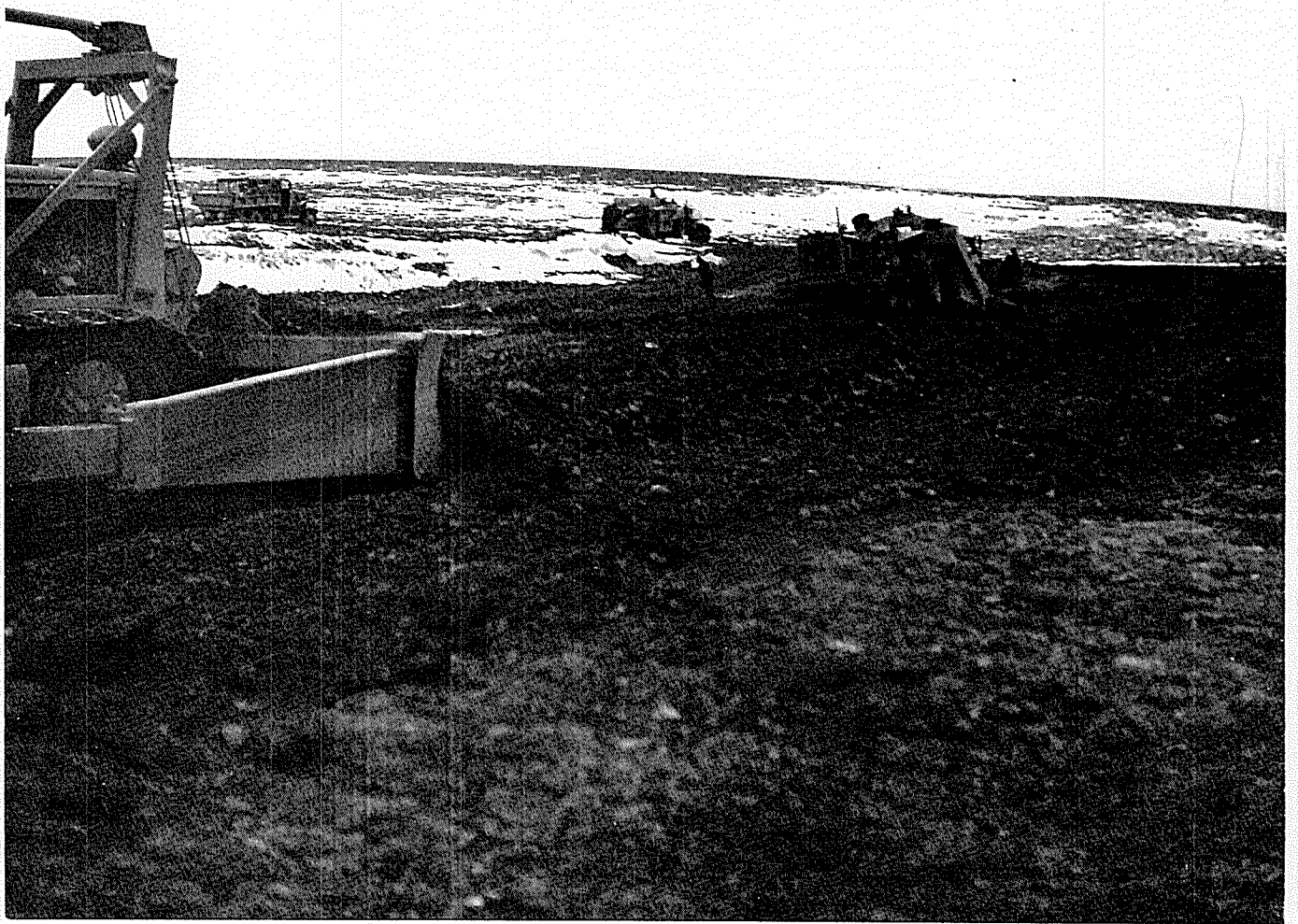


Fig. 33-D : US 97 Home; equipment working  
borrow pit.



Fig. 33-B M3 67 1/2-ton - Adams motor patrol.



Fig. 33-P US 97 - Nons. Typical fill section placed by contractor.

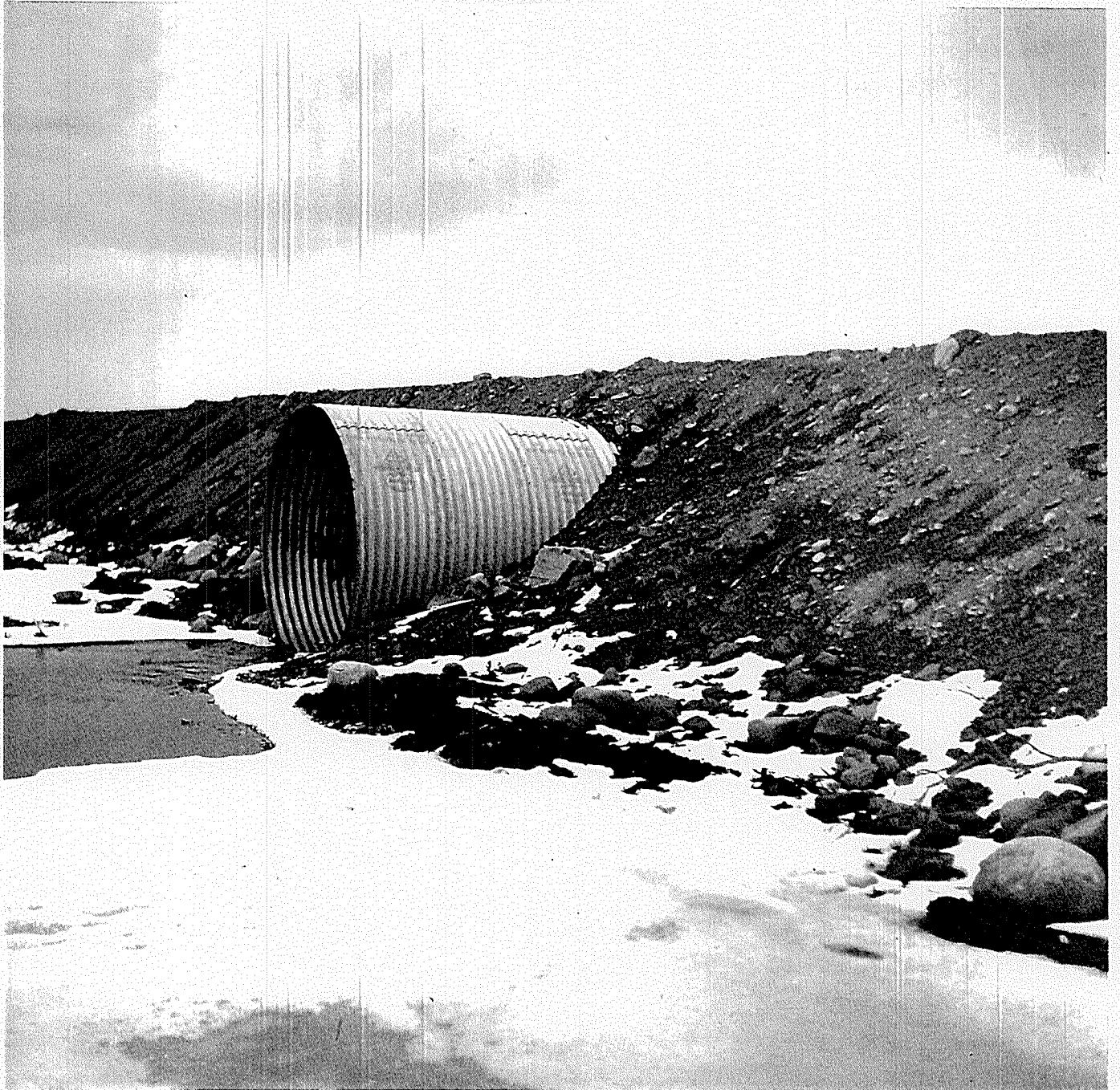


Fig. 33-4 US 97 Stone - 60-inch  
multi-plate CIP culvert.



Fig. 35-II - US 97 team - Rear camp.  
at Center Creek.



Fig. 33-1 US 07 1940 - maximum fill.



Fig. 33-J US 97 Home - 10-foot multi-plate CMP culvert.



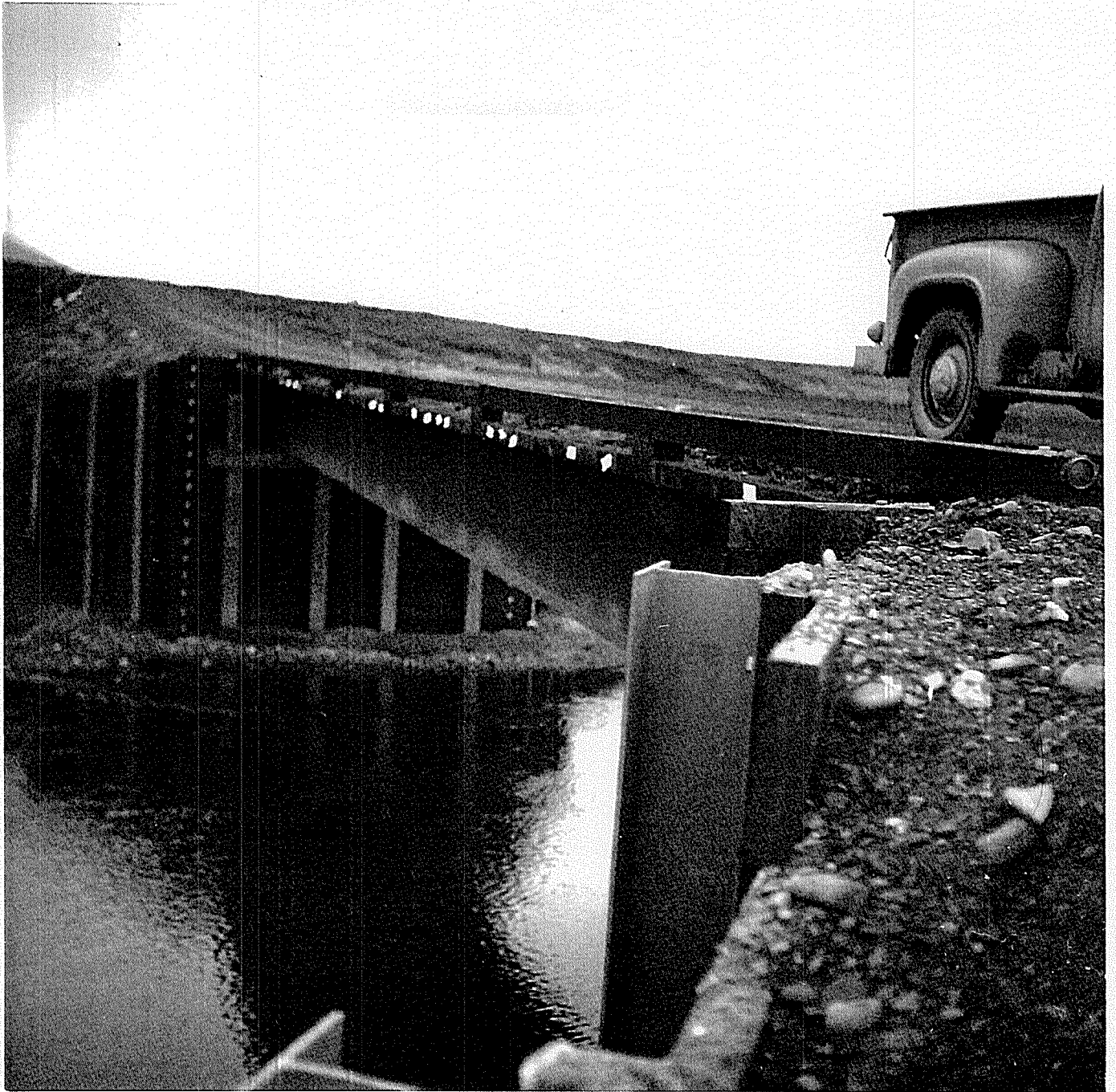


Fig. 33-1 US 97 News - Bridge over  
Grand Central River.

34. NOME - SOLOMONLocation

This road begins at Nome; thence easterly along the beach line to the Solomon River; thence northerly along the Solomon River.

Physical Data

The work this season consisted of the construction of a bridge and approaches, eliminating a ferry across the ocean inlet near Dickson, and the improvement of approximately 5 miles of the river road.

The bridge consists of a 2-span plate girder with a timber deck and approximately 300 feet of embankment at the north approach. The fill material was being hauled approximately 3.2 miles from a pit in the Solomon River.

The road improvements consist essentially of building up fills at intermittent locations along the gravel bars and river gravel benches, and shaping the higher areas. Most of this work was done with a dozer merely robbing the ditches to build the fill and may be considered a dozed trail. However, in some areas it was necessary to haul in fill material. There are some shallow V ditches on the high side of sidehill sections but much of the road may be considered untrained. The aggregate total length of improvements completed this year is estimated at approximately 5 miles. The width varies, but most of the road is one lane. The road generally follows the alignment and contours of the river bench and can be classified as a low rolling grade. The maximum gradient on improved sections was approximately 8% and the maximum fill noted was about 16 feet in depth. The average fill on the improved portions of the road is approximately 1 1/2 feet.

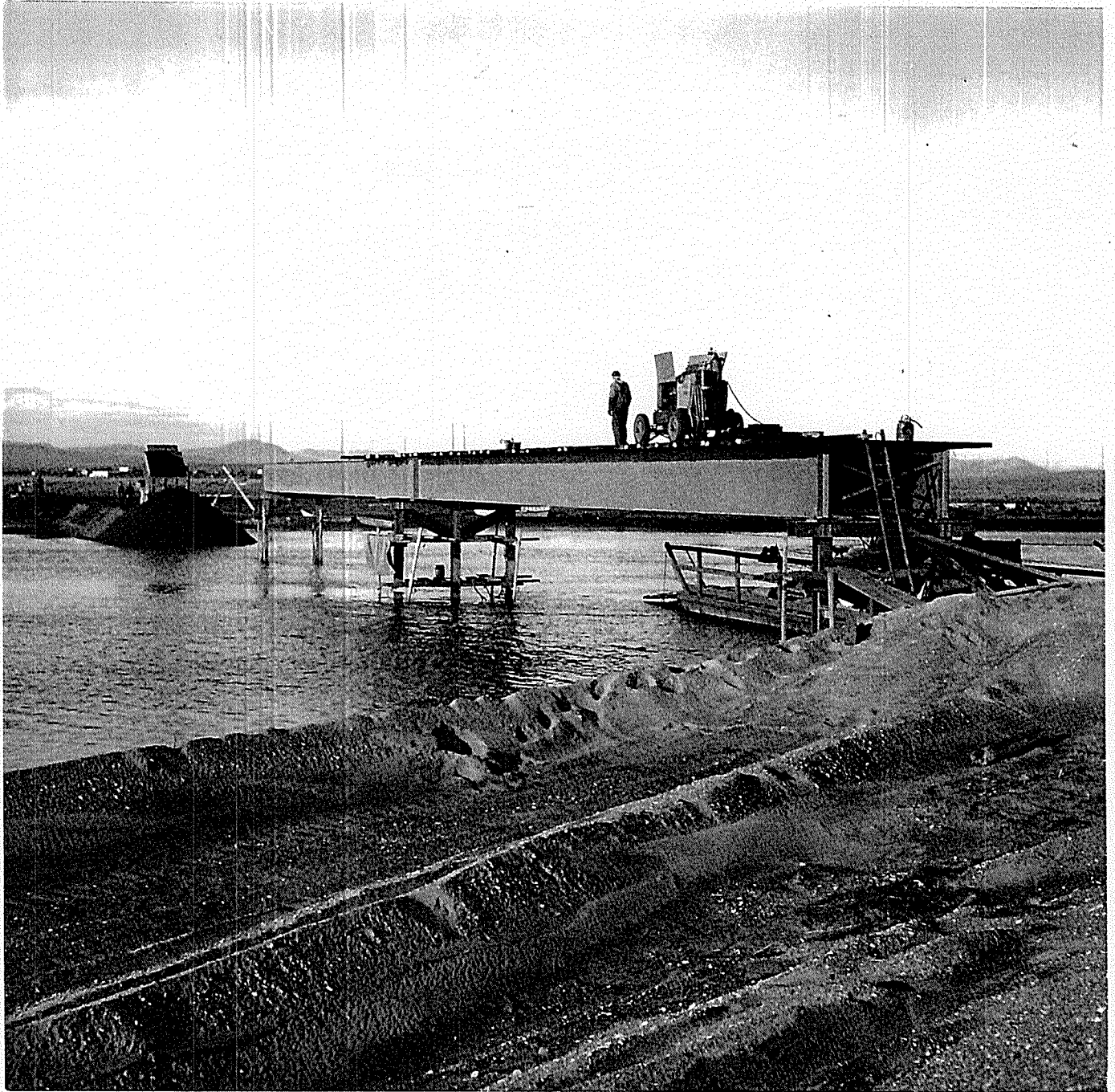
The only construction activity in progress at the time of the writer's visit (October 3) was the bridge construction. The following equipment was observed at work:

- 4 - Dump trucks
- 1 - Crawler crane
- 1 - Caterpillar D-6 with front end loader
- 2 - Caterpillar D-8
- 1 - Caterpillar #12 motor patrol

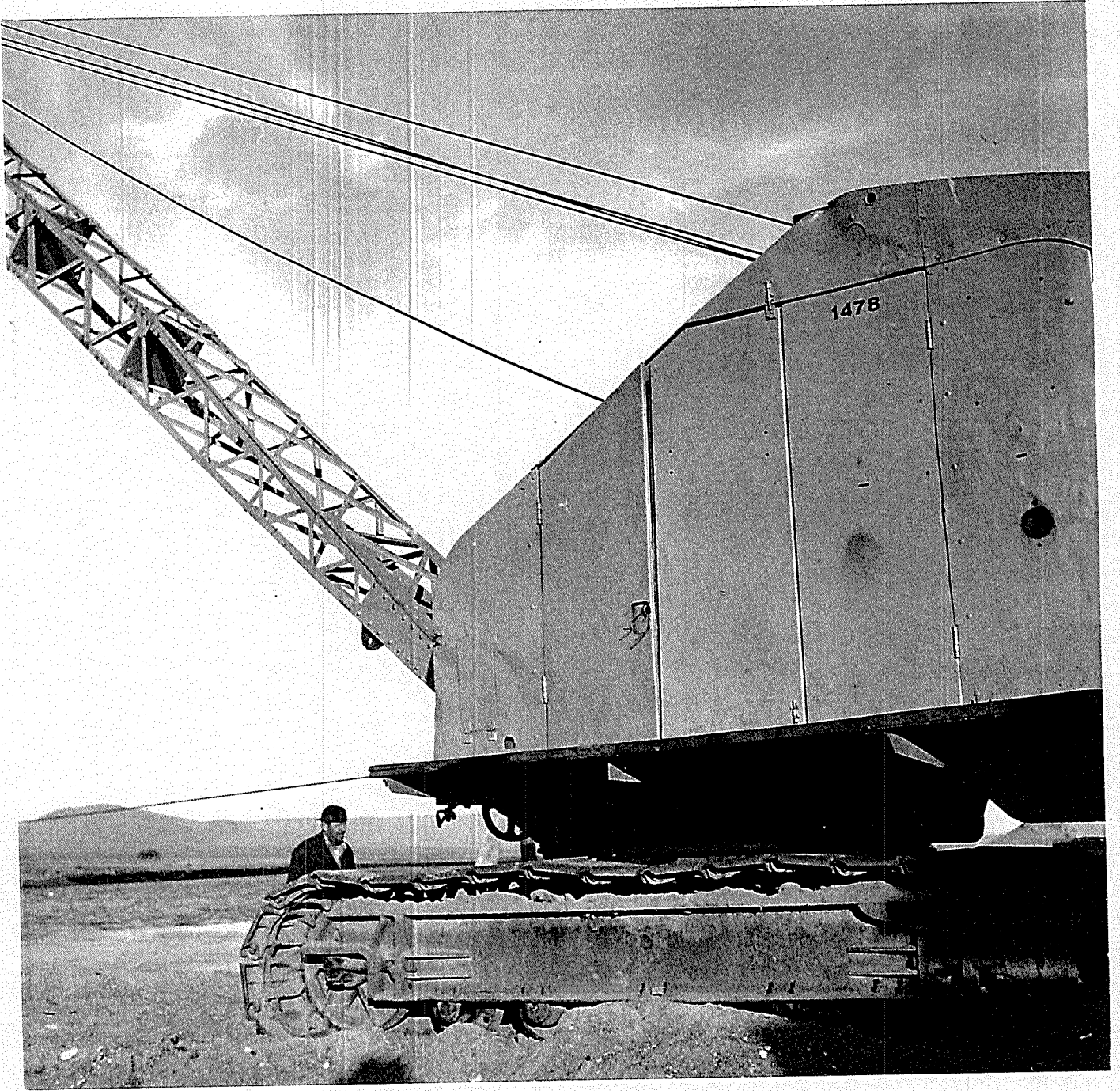
Work reputedly started in May on this project.

The standards on this project are low. Much of the road will probably be lost during periods of high water, and it appears that the bridge should have an additional one or two spans for economic reasons, as well as maintenance. It further appears that the alignment and gradient of the road are dictated by the meandering of the river rather than any established standards.

There are several small and large contractors with equipment in the Nome area capable of performing this work.



**Fig. 34-A** **Ham-Solomon; bridge construction**  
**over ocean inlet near Dixon.**



Plt. 34-B

Hono-Solomon; crane used for erecting plate girder bridge.



Fig. 34-C New-Solomon; long embankment construction at north bridge approach.