



FIG. 31-D Home Spit Road -
beach on beach.
on beach.

32. KODIAK

Location

This project is a connecting road between the beach road and the road that continues on to Mill Bay. The road crossover is to the northeast of Beaver Lake which is north of the townsite of Kodiak.

Physical Data

The road is estimated at 0.8 mile in length. Approximately 0.6 mile of heavy clearing and grubbing 50 feet in width and 0.1 mile of fill with a roadbed width of 18 feet to 20 feet shoulder to shoulder had been completed at the time of the visit (October 16). The road traverses rolling topography, and on the section completed the maximum estimated gradient is 3%, the maximum fill is 5 feet and the average fill is approximately 3 feet. Fill is hauled from a rock quarry a distance of approximately 2 miles north on the Mill Bay road.

Drainage structures consist of corrugated metal pipe culverts; the placing of one was observed (see photographs). There has been no excavation to date. Logs and stripplings are doxed into low areas and used for embankment as well as bedding and backfill around the culvert.

Statistics

The major pieces of equipment located on the project were:

- 2 - Dump trucks
- 1 - Caterpillar D-7
- 1 - Caterpillar D-6
- 1 - Compressor with 2 drills

There were 7 men on the project at the time of the writer's visit. Clearing was started the middle of September and hauling from the pit was started the first of October.

Contractors with equipment spreads located on Kodiak were Russell Wright, Morrison-Knudsen Company, Inc., and several contractors on the Naval Base.



Fig. 32-A D-7 Cat clearing and stripping.

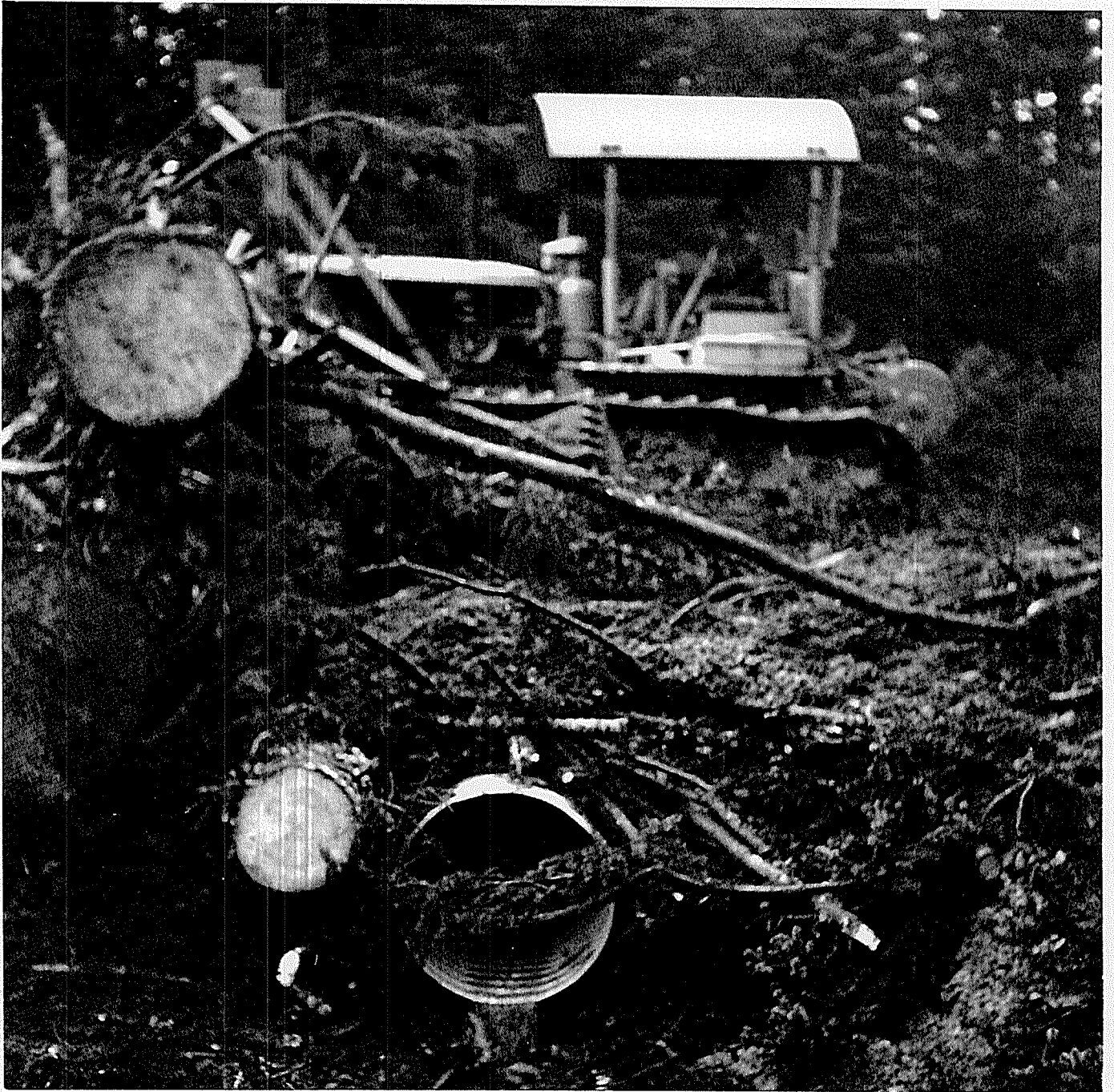


FIG. 32-B Kordak - backfilling CMP
culvert with logs and strippings.



Fig. 32-C Kodiak - end of cleared section.



Fig. 32-D Kodiak - heavy clearing and grubbing necessary.



Fig. 12-B Kodint - end of road looking east.

1964-1965
1966-1967
1968-1969
1970-1971
1972-1973
1974-1975
1976-1977
1978-1979
1980-1981
1982-1983
1984-1985
1986-1987
1988-1989
1990-1991
1992-1993
1994-1995
1996-1997
1998-1999
2000-2001
2002-2003
2004-2005
2006-2007
2008-2009
2010-2011
2012-2013
2014-2015
2016-2017
2018-2019
2020-2021
2022-2023
2024-2025



Fig. 32-F Kodiak - Two men drilling and shooting at quarry.



Fig. 32-6 Kodiak - Equipment at Quarry.

33. US 97 - NOME EAST

Location

This project is a section of the proposed highway from Fairbanks to Nome which is part of the Federal Aid Highway System and designated US 97. The road begins at Nome, thence North along the general route of the abandoned Seward Peninsula Railroad to Salmon Lake; thence in a northeasterly direction along the general route of and north of Salmon Lake and the Kruganapa River to Golden Gate Pass; thence northwesterly across the lowlands to Pilgrim Creek. Construction this season was performed on approximately 30 miles between Salmon Lake and Pilgrim Creek.

Physical Data

The construction of this road entails the use of three parties. The advance party currently (October 1) located at Pilgrim Creek consists of three D-8 Caterpillars with angle dozers. There is very little clearing involved. The angle dozers start at the ditch line and work the tundra and shallow ditch excavation material toward the center of the road. The tundra under the roadbed is left undisturbed. The advance party also locates borrow pits.

The second party, currently located at Golden Gate Pass approximately 11 miles behind the advance party, consists of earth-moving equipment which fills in low and unstable portions of the road.

The third party consists of the S & B Construction Company with a fleet of 8 dump trucks which haul pit borrow material and end dump a gravel course approximately 1 1/2 feet thick on the road sections that were skipped by party #2.

The advance party commenced operations in May and covered a distance of approximately 30 miles. Party #2 has covered a distance of about 19 miles, and party #3 has covered approximately 18 miles. However, they have worked on only approximately 11 of the 18 miles. Therefore, approximately 18 miles have been completed this season, except for a bridge across Crater Creek

and 30 miles are partially completed.

The roadbed width is 24 feet and the maximum cut is approximately 6 feet. The maximum fill is approximately 20 feet in depth and the average fill approximately 1½ feet.

The road traverses the low, rolling foothills of the Kigluak Mountains and generally follows the contours except at creek crossings.

In addition to the small culverts, the drainage structures consist of large multi-plate, corrugated metal pipe culverts and a plate girder span across the Grand Central River.

Statistics

Equipment noted on the project was as follows:

Bureau of Public Roads equipment

- 7 - Caterpillar D-8
- 2 - International TD-18
- 5 - Scrapers
- 1 - Adams motor patrol

H & B Construction Company equipment

- 6 - Dump trucks
- 2 - Caterpillar D-8
- 1 - Front end loader

The total number of men on the project was approximately 22. This is an estimate since the writer was unable to reach the advance party.

The standards to which this road is being built appear to be the same as mentioned in Sections 1 and 2, Livengood - Bureha.

There are numerous small contractors in the Nome area and several with large equipment spreads. The largest of these are Morrison-Knudsen Company, Inc. and BUCK-McLaughlin. The project is accessible by highway to Nome.



Fig. 32-A US 97 Home; cat and can with
push cat working borrow pit.



116 97 Heavy tank with engine
ready to transport.